



Grand Canal Dock

Amending Planning Scheme, July 2006

Grand Canal Dock Planning Scheme (Amending Planning Scheme No.1) 2006

Amending Planning Scheme

Incorporating Ministerial Modifications

Made by Dublin Docklands Development Authority on the 12th January 2006 & Approved by the Minister for the Environment, Heritage & Local Government on the 26th day of June 2006

Note:

This Amending Planning Scheme must be read in conjunction with the Grand Canal Dock Planning Scheme, 2000.

Dublin Docklands Development Authority

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Council

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John Boylan

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Department of the Environment, Heritage and Local Government

Dublin Docklands Development Authority Act, 1997

The Minister for the Environment, Heritage and Local Government pursuant to subsection (5) of section 25 of the Dublin Docklands Development Authority Act, 1997 (No. 7 of 1997) (hereinafter called "section 25") hereby approves, subject to the following modifications, the planning scheme for the Grand Canal Docks Area (the area as described in the Schedule beneath), which scheme was submitted to him by the Dublin Docklands Development Authority (hereinafter called "the Authority") on 10th March 2006:-

Modifications

- 1. Insert the following as the final paragraph in section 3.1.2 (Residential):***

"The Authority will require all section 25 applications for residential development (except in the case of south-facing elevations or where directly opposing windows are more than 22 metres apart) to be accompanied by daylight, sunlight and shadow projections to demonstrate that reasonable standards of residential amenity can be achieved."

- 2. Insert the following as the final sentence in section 3.2.3 (Scale and massing):***

"Designers will be required to articulate clearly public, semi-public and private spaces, and to protect the privacy of semi-public and private spaces where they directly abut public streets."

- 3. Substitute the second sentence in section 3.3.3 (New urban space) with the following:***

"It will be located towards the eastern side of the site."

Schedule

In this Schedule –

Unless otherwise specified, any reference to a line drawn along any bridge, railway track, river, road or street, shall be construed as a line drawn along the centre of such bridge, railway track, river, road or street as the case may be.

That part of the county borough of Dublin bounded by a line commencing at the point (hereafter in this description referred to as the first-mentioned point) where the River Liffey is intersected by the northerly projection of Lime Street, then continuing in a southerly direction along the said projection and Lime Street to the point where it joins Hanover Street East, then continuing in an easterly direction along Hanover Street East to the point where it joins Macken Street, then continuing in a southerly direction along Macken Street to the point where it is intersected by Pearse Street, then continuing in an easterly direction along Pearse Street to the point where it is intersected by Grand Canal Quay, then continuing in a southerly direction along Grand Canal Quay to the point where it is intersected by the railway track, then continuing in a south-easterly direction along the said railway track to the point where it intersects Barrow Street, then continuing in a northerly direction along Barrow Street to the point where it joins Ringsend Road, then continuing in an easterly direction along Ringsend Road to the point where it is joined by South Dock Road, then continuing in an northerly direction along South Dock Road to the point where it is joined by an unnamed laneway (known locally as Bottle House Lane), then continuing

initially in a south-easterly direction along the said unnamed laneway and the easterly projection thereof to the point where it intersects the northern projection of the eastern boundary of Camden Lock apartments, then continuing in a southerly direction along the last mentioned projection and boundary and the southern projection of the said eastern boundary to the point where it intersects Ringsend Road, then continuing in an easterly direction along Ringsend Road and Ringsend Bridge to the point where it is intersected by the eastern boundary wall of the River Dodder, then continuing in a northerly direction along the said eastern boundary wall to the point where it is intersected by the western projection of the northern boundary of the O'Rahilly House flats, then continuing in an easterly direction along the last mentioned projection and boundary to the point where it joins the western boundary of the O'Rahilly House playground, then continuing initially in an northerly direction along the western and northern boundaries of the said playground and the easterly projection of the last-mentioned northern boundary to the point where it intersects Thorncastle Street, then continuing in an northerly direction along Thorncastle Street and the northerly projection thereof to the point where it intersects the River Liffey, then continuing in a westerly direction along the River Liffey to the first-mentioned point.

***GIVEN under the Official Seal of the Minister
for the Environment, Heritage and Local Government this
26th day of June 2006.***

***Dick Roche
Minister for the Environment, Heritage and Local Government***

Grand Canal Dock Planning Scheme
(Amending Planning Scheme No.1) 2006
Amending Planning Scheme

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This Amending Planning Scheme must be read in conjunction with the Grand Canal Dock Planning Scheme, 2000.

NOTE:

This document incorporates the modifications made by the Minister and, where appropriate, the modifications have been highlighted, italicised and bold text. Where a conflict arises between the Minister's modifications and the original content and diagrams of the planning scheme, the modifications take precedence".

1.1 Background

The Grand Canal Dock Planning Scheme, 2000 was made by the Dublin Docklands Development Authority on 25th July 2000 and approved by the Minister for the Environment and Local Government on 21st December 2000.



This Amendment of the Planning Scheme is being made in accordance with the scope of the legislation governing the making of Planning Schemes.

1.2 Purpose of the Amendment

The purpose of the Planning Scheme is to provide robust and detailed direction for the planning and development of the relevant part of the Grand Canal Docks. The specific purpose of the Planning Scheme Amendment is:

- To recognise the existing and changed development context in the area
- To acknowledge the significant potential of this strategic and prominent site, and
- To provide more detailed guidance for its planning and development

The Authority carried out focussed consultation with relevant agencies and parties, in accordance with the relevant statutory requirements, during the preparation of this Amendment. The accompanying EIS assesses the environmental effects of the Scheme, including those associated with the tall building.

1.3 Content

This document outlines key amendments to the Grand Canal Dock Planning Scheme, 2000. The amendments are site specific, relating to the area described below in paragraph 2.1, and must be read and interpreted in conjunction with the Grand Canal Dock Planning Scheme, 2000, and the policies and objectives of the Dublin Docklands Master Plan, 2003. The amendments will be outlined under the following broad headings:

- The nature and extent of proposed development
- The proposed distribution and location of land uses
- Proposals in relation to the overall development, including the maximum heights, and the external finishes of structures

- Proposals relating to development of amenities and the conservation of the architectural heritage or other features
- Proposals relating to transportation, including the roads layout, the provision of parking spaces and traffic management

The definitions contained in the current Planning Scheme, apply to this amendment.

1.4 Planning Policy

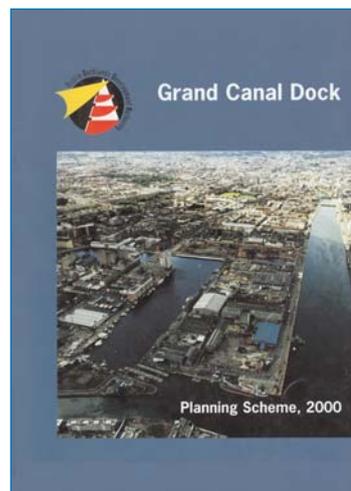
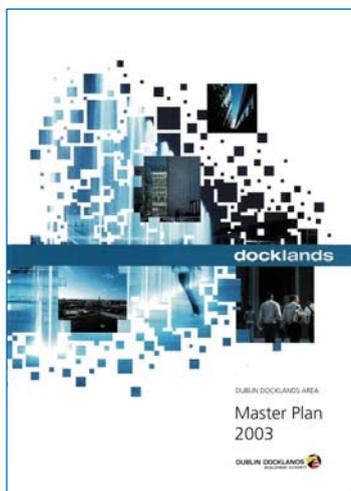
Relevant planning policy, additional to, or revised from that outlined in the Grand Canal Dock Planning Scheme, 2000, is set out below.

1.4.1 Dublin Docklands Area Master Plan, 2003

The Master Plan outlines the broad strategic and planning context, policies and objectives for the Docklands Area. Important objectives for the area are included in the Master Plan.

The land use objective for the Campshires is Zone 9, "To preserve, provide and improve recreational amenity and open space". The zoning objective for the remainder of the area is Zone 14, "To seek the social, economic and physical regeneration of an area with mixed use of which residential and Zone 6 would be the predominant use". The water body is covered by Zone 11, "To protect and improve canal, coastal and river amenities".

The Master Plan indicates residential, office and commercial and enterprise uses on the site in mixed use areas. A proposed hotel/hotel cluster is shown on the site as part of the tourism, leisure and cultural objectives of the Master Plan. Retail objectives include ground floor retail to Sir John Rogerson's Quay and retailing to Grand Canal Dock at Britain Quay. A Dodder bridge is included in the transport objectives. The Master Plan requires this bridge to be public transport, pedestrian and cycle only.





The Master Plan outlines the Guiding Principles of Civic Design under the headings of context, variety, permeability, legibility, movement, movement spaces, scale and height, architectural design and open spaces. Notably, the guidelines identify potential for permeability in new developments and the importance of proportionate building scale and height to spaces. In architectural terms, the importance of the design of corners and entrances is emphasised. Sir John Rogerson's Quay is identified as the eastern extent of an important new east-west connection across the city centre, connecting Heuston Station and the subject area. The site is identified as an appropriate location for a high building. Significantly, Policy 11 under paragraph 6.1.3, states, the Authority will "seek a coherent architectural expression to both sides of the river in order that the Liffey Corridor can be read as an entity".

The Master Plan includes the designation of the Liffey Corridor and Grand Canal Dock as a Conservation Area. Its broad conservation policies recognise the importance of the underlying streetscape and building pattern. Notably, the orthogonal grid street pattern is present in the subject area. The Master Plan also refers to the two Protected Structures within the Area: the Hailing Station on the Campshire, at the corner of Sir John Rogerson's Quay and Britain Quay, and the walls, setts, mooring rings, steps and bollards of Sir John Rogerson's Quay. The Master Plan recommends the deletion of the Hailing Station from the Record of Protected Structures.

The Campshires and Grand Canal Dock waterfront are designated as linear parks under the Amenity Objectives.

1.4.2 Dublin City Development Plan 2005-2011

The Master Plan and the Dublin City Development Plan are required to be consistent in terms of policy and objectives. The City Development Plan reflects the broad objectives and policies contained in the Master Plan for this area, including the provision of the Dodder Bridge as a public transport/pedestrian only bridge.

1.4.3 Other relevant plans and policy

Regard will be had to other relevant plans and policy relating directly or indirectly to the subject amendment including: A Platform for Change, Strategy 2000 - 2016, DTO, the Retail Planning Guidelines, the Residential Density Guidelines, the Social Housing Design Guidelines, the Childcare Facilities Guidelines, the Architectural Heritage Guidelines and the Dublin City Council's Part V Housing Strategy.

2.1 The Area

The subject area of this Amendment includes Area 4, as described in the Grand Canal Dock Planning Scheme, 2000, and a large part of the water body surrounding it to the north and east (the confluence of the Liffey, the Dodder and the Grand Canal). Area 4 is bounded by Sir John Rogerson's Quay, Britain Quay, Green Street East and Benson Street. It includes one significant urban block (referred to hereafter as the main site) measuring approximately 1.65 ha. and a smaller wedge shaped site, excluding the existing Campshires, on the Britain Quay/Sir John Rogerson's Quay Campshire (referred to hereafter as the smaller site), measuring approximately 0.25 ha.

The main site has been cleared of buildings and structures. The smaller site is also cleared and includes the former Hailing Station.

Option 1 in this Planning Scheme allows for the development of the two sites separately, while Option 2 allows for a combined development.

2.2 Urban Analysis

The area occupies a strategic and prominent position on an important gateway to the City and near an important crossing of the river. The area also marks a significant change in character and urban development as the evolving docklands meet the port.

In visual terms, the area is positioned at the widening of the Liffey water body and its confluence with the Dodder and the Grand Canal. The waterbody provides an important visual setting as well as providing relatively long and unobstructed views to the waterfront.

The prominent location of the site will ensure that any significant development of the site will be high profile in nature. The potential for marking the area with a landmark/higher building has been recognised in the current Planning Scheme. The Master Plan identifies a larger legibility

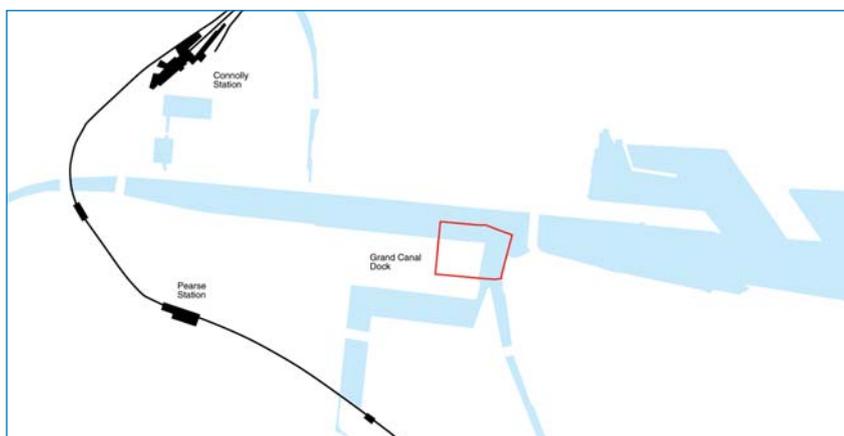


Diagram 1. Site Context

objective for this landmark building, acting with other planned high buildings in the Docklands (Docklands North Lotts/Point Square and Grand Canal Docks) as a significant and visual point of reference. Locally, the landmark building will need to provide legibility and position, while its scale should be such that it remains part of the surrounding urban fabric. The final development outcome will need to consider relationships with the planned development on the opposite sides of the water bodies.



2.3 Development Context

Planning permission was granted for a mixed use development of the site in 2002. The proposal consisted of four smaller, orthogonal blocks within the site. The proposal provided for a part podium at ground floor level and a landmark tower, measuring 95 metres in overall height above street level, on the corner of Sir John Rogerson's Quay and Britain Quay. This permission has not been implemented.

A competition was held in 2002 for the design of a landmark tower and studio on the smaller site. The winning entry proposed a 60m tower, with studio at the top levels and an associated plinth building.

Significant adjoining and adjacent developments have now been certified in accordance with the Planning Scheme. The redevelopment of the "Chocolate Factory" site on the west side of Benson Street has been certified and is proceeding. Immediately west of the subject site, the new urban square at Benson Street is at design stage and will be developed shortly. The mixed use development to the south on the end/corner site at Green Street East/Britain Quay/Hanover Quay has been certified in accordance with the Planning Scheme.



The improvement of the Campshire has been completed to the end of Sir John Rogerson's Quay. Design proposals for a public transport bridge spanning the mouth of the Grand Canal Dock and Dodder, between Sir John Rogerson's Quay/Britain Quay and York Road in Ringsend, are under consideration. New development shall have regard to the provisions and recommendations of the Greater Dublin Strategic Drainage Study in relation to flood protection measures.

Unless explicitly so stated, the existing policies and provisions of the Grand Canal Dock Planning Scheme, 2000, remain in effect. Additional policies and provisions have been included, reflecting other updated policies which, post-date the original Planning Scheme.



3.1 The nature and distribution of development and uses

3.1.1 Introduction

Diagram 2 shows the proportionate emphasis on land use with the light blue being predominantly residential and the darkest blue being commercial offices. The principle of mixed development applies throughout the Amendment area.

3.1.2 Residential

Residential development should be focussed on Green Street East and Benson Street. It should also form an important part of mixed use development to Sir John Rogerson's Quay and Britain Quay. Paragraph 15.9.2 and 15.9.3 of the Dublin City Development Plan 2005-2011 relating to the size and layout of units shall, as a minimum, apply to new development in addition to the general provisions of the Social Housing Guidelines, or any other applicable Ministerial guidelines that may supercede these guidelines. Development shall be designed to protect amenities, including privacy, and shall be laid out to secure adequate daylight and sunlight. The Authority will ensure adequate internal and external layout of apartments and seek to ensure that a minimum of 25% of all units shall be family sized, with a minimum floorspace of 85 sq m.¹ "Own front door" townhouses and owner-occupier and family type units will be promoted on Green Street East. Apartments will provide for

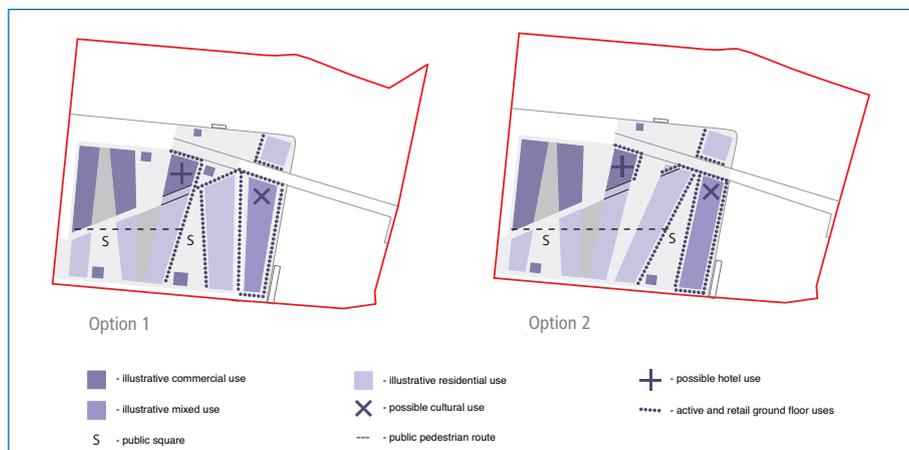


Diagram 2 - Land Use Concepts

¹ Section 4.2.5 of the Dublin Docklands Area Master Plan, 2003

dual aspect. In appropriate circumstances, such as at difficult corner locations, single aspect units with a western, southern or eastern aspect may be considered. The Planning Scheme requires, in respect of residential development, at least 8 square metres per bed space of private or semi-private open space. The Authority will, however, in assessment of Section 25 applications, where it relates to a residential use within the landmark tower, be prepared to reduce this requirement if it can be demonstrated by the applicant that there is sufficient public open space. Residential development shall have regard to existing noise nuisances in the area and sound insulation measures shall be provided for new residences as appropriate.

“The Authority will require all section 25 applications for residential development (except in the case of south-facing elevations or where directly opposing windows are more than 22 metres apart) to be accompanied by daylight, sunlight and shadow projections to demonstrate that reasonable standards of residential amenity can be achieved.”

3.1.3 Office and enterprise use

Small ground floor business having a significant public use such as estate agents, photocopying, car-hire, recruitment offices or similar will be encouraged in areas where animation of the public realm is important, as shown in Diagram 2.

3.1.4 Retail

Small and medium size retail will be required at ground floor level on the main frontages of the development and on the proposed new square where animation of the public realm is important, as shown in Diagram 2. The number and frequency of retail units should be maximised to reflect a fine grain of development at ground floor level. Suitable uses will include local convenience, small specialist retail, restaurants and cafés, and associated office support such as reprographics and supplies.

3.1.5 Special uses

The proposed new landmark building is to incorporate a new music studio. The studio is likely to occupy the top levels of the building. The development of ancillary and associated uses in the area such as a multi-media arts centre, performing arts venue or other cultural activity should be provided within the vicinity of the square. The Authority will also have regard to the requirements of the Childcare Facilities Guidelines for Planning Authorities.²

3.2 Overall Design

3.2.1 Urban and block structure

A variation to the orthogonal block pattern of the Planning Scheme is

² Section 4.2.6.12 of the Dublin Docklands Area Master Plan, 2003

appropriate to the area under amendment to articulate the end of the peninsula and facilitate an intensification of activities at a new focal point and public realm. A new diagonal street will traverse the main site connecting the new square at Benson Street and the proposed new urban space and landmark building at the corner of Sir John Rogerson's Quay and Britain Quay. The acceptable general variations on block structure are shown in the accompanying plans and sections.

3.2.2 Urban grain

The block structure will optimise block size and will allow for a series of north-south linear blocks at offsets to give maximum light penetration and vista to the waterfront. Penetrated by the diagonal route, the blocks will clearly distinguish between public and private space. All block frontages will be required to front public spaces and streets. The larger block frontages to Benson Street, Sir John Rogerson's Quay and Britain Quay shall be articulated to provide for a finer grain of development. This will require consideration of block subdivision into smaller plots, typologies and uses.



Diagram 3 - Urban Structure

3.2.3 Scale and massing

The scale of the urban blocks will be related to position within the overall hierarchy of streets and spaces in the area and the desired use and character of new streets and spaces. The principal frontages to Sir John Rogerson's Quay and Britain Quay will present a larger grain and scale of development, reflecting their prominence and position. In addition to the layout illustrated in Diagrams 3 and 5, the Authority will consider acceptable the closure of one or more of the blocks fronting onto Sir John Rogerson's Quay. The scale of development along Benson Street will reflect permitted and ongoing development on the street and its function as a significant street in the area. The scale of development to the proposed new urban square will reflect the scale of the space and the need to maximise sunlight and daylight. The scale of buildings fronting the new streets within the site will be modest, to reflect an intimate human scale that is appropriate to living, residential streets. Building massing will be placed principally along the perimeters of the new blocks, providing for continuous street and space frontage and enclosed private/shared amenity spaces within. ***“Designers will be required to articulate clearly public, semi-public and private spaces, and to protect the privacy of semi-public and private spaces where they directly abut public streets.”***

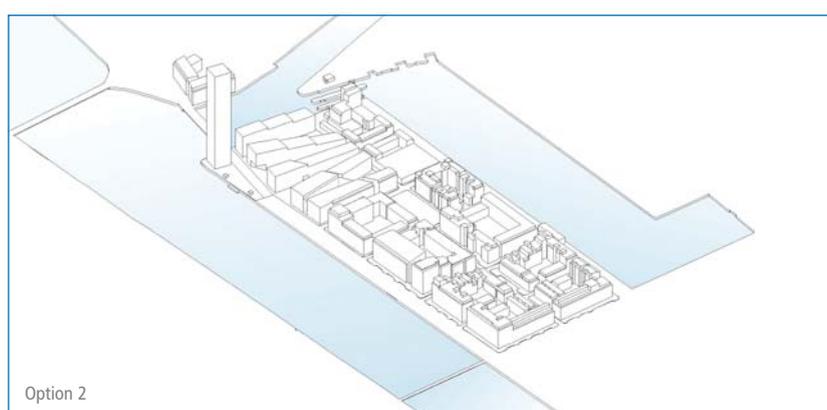
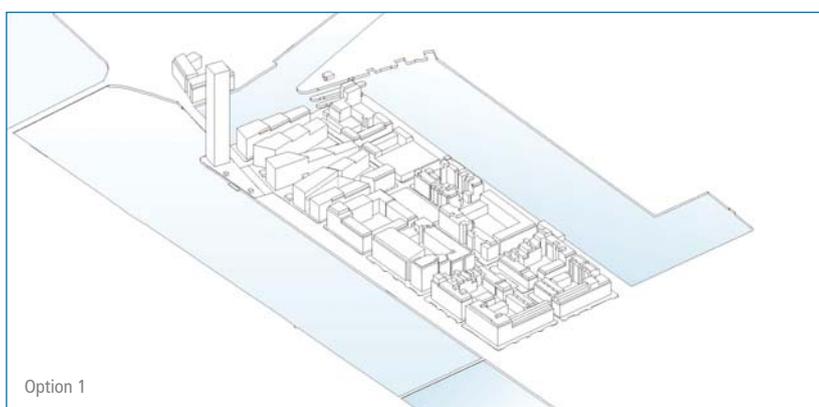
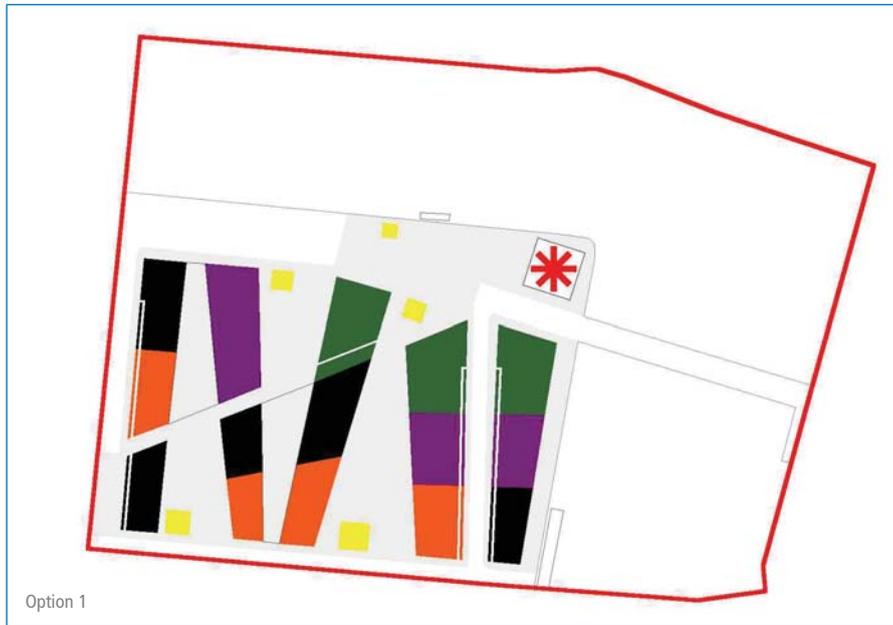


Diagram 4 - Indicative Views



-  - 8 storey commercial or 9 storey residential
-  - 7 storey commercial or 8 storey residential
-  - 6 storey commercial or 7 storey residential
-  - 5 storey commercial or 6 storey residential
-  - 1 storey commercial
-  - High landmark building
-  - Top storey setback indicated thus

Diagram 5 - Block Heights

3.2.4 Building height

Building heights within the blocks will vary in order to achieve appropriate urban scale. Maximum numbers of storeys are prescribed for both commercial and residential development as shown in Diagram 5. The controls will allow for vertically mixed use buildings with commercial, residential and other use storeys. The main element of the landmark tower should not exceed 100 metres in height to the shoulder above existing street level. The shoulder is the top of the front wall of the building, excluding any parapet. Accommodation above this level must be well set back and consistent with architectural and service elements. Such elements will be permitted subject to a maximum overall building height not exceeding 120 metres above existing street level. As the surrounding streets lie below recommended flood levels, developers should inform themselves of suitable ground floor levels to ensure flooding is avoided. The existing street level is not consistent, therefore overall building height above existing street level may vary. The relationship between the development and the street at ground floor level will need to be carefully considered.

The prescribed maximum building heights are shown in the accompanying plans and sections. Plot ratios should fall within the indicative range outlined in the Dublin City Council Development Plan, 2005, and subject to the provisions of Paragraph 15.4.0 of the Development Plan.

3.2.5 Architectural design

3.2.5.1 Roofscape

The proposed development will express variety and interest at roof level. The set-back storey permitted above building shoulder height will be designed to provide interest and variety of roofscape. Changes of roofscape should reflect different building typologies and uses. Architectural features having non-useable floorspace above the maximum recommended heights will be considered on a case-by-case basis.

3.2.5.2 Disabled access

All buildings and spaces will be designed for ease of access and use by the disabled.

3.2.5.3 Landmark tower

A landmark tower will be located towards the north-east corner of the area in a position indicated in the accompanying plans and sections. For reasons of good planning and landmark quality, the Authority considers it appropriate that the landmark tower be located in the immediate vicinity of the junction of Sir John Rogerson's Quay and Britain Quay, to take account of this specific area's strategic setting at the north eastern corner of the Area. In this regard, the siting of this landmark tower, for the purpose of this amending Planning Scheme, is site specific.

The requirements for such a tower would not be satisfied by any other landmark tower that may be permitted or constructed in the Grand Canal Dock Area. The tower will be located in a position that will allow the construction of the public transport bridge and access road, and adequate public access to the extended Campshire at Britain Quay. The overall slenderness ratio for the tower will not be less than 4:1. The footprint will need to have regard to the need for public access to the waterfront. The ground floor of the tower should present active uses to surrounding spaces. The architectural design of the tower should show clear consideration of the base, shaft and capital elements.

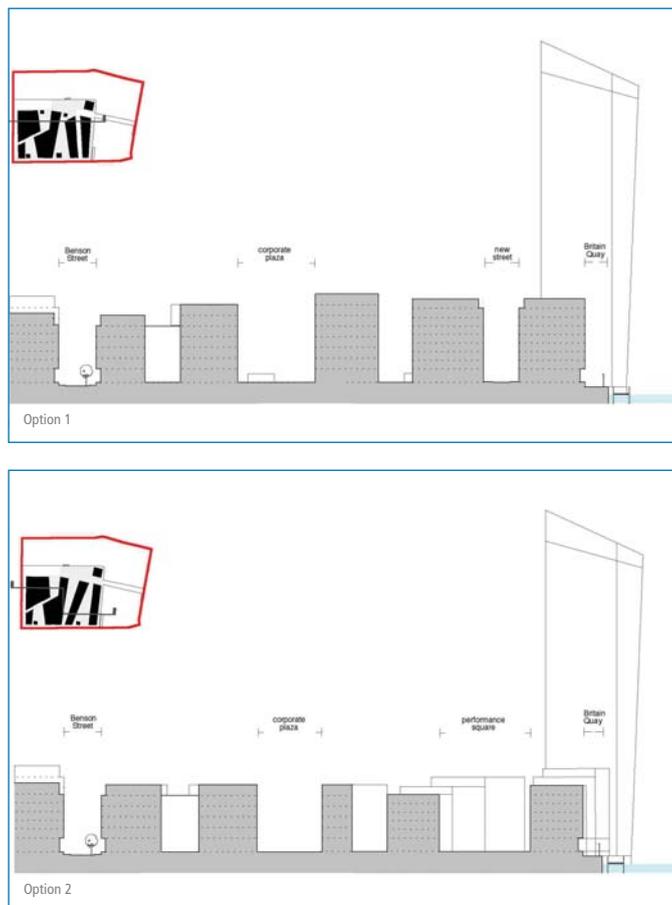


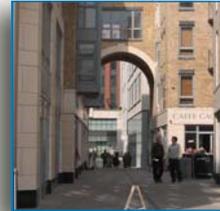
Diagram 6 - Site Sections

3.2.5.4 Bridge design

In design terms, the bridge structure should be elegant and slender in proportions, minimising visual impact on the water body and the Conservation Area. The supporting elements, the deck and the construction materials will need to be designed and specified to a high aesthetic quality. The bridge shall be a lifting or swinging opening bridge with closed air draft to permit small leisure craft.³ The bridge will also be designed to accommodate LUAS which may serve the Poolbeg Peninsula.

³ Section 5.1.8.19 of the Dublin Docklands Area Master Plan, 2003

3.3 Development of amenities including conservation of heritage



3.3.1 Public space and amenities

The quality of public space is of paramount importance in the development of this area. All streets and urban spaces should be considered in a comprehensive way, prioritising pedestrian use, quality of design and finishes. The connections between streets and spaces will need to be carefully handled. Level changes should be minimised, with emphasis placed on continuous surfaces.



3.3.2 The Campshires

The Campshires will be extended from Sir John Rogerson's Quay along the Britain Quay waterfront as an integral part of the development. The nature and extent of the extended Campshire will depend on the development option pursued. In any event, the extended Campshire will need to provide for comfortable pedestrian access and use. The Authority will promote the development of small kiosks and pavilions on the existing Campshire at Sir John Rogerson's Quay to support recreational and leisure uses. Provision will be made for a water taxi landing on this Campshire. The existing Hailing Station will be conserved by record and demolished or, if appropriate, relocated. The development of Britain Quay will allow for lay-by moorings.



3.3.3 New urban space

A new urban space will be developed on the site. ***“It will be located towards the eastern side of the site”***. Its general size and proportions are shown in the accompanying plans and sections. The space will be developed to the highest design standards, ensuring quality and robustness. It will need to provide for a wide range of uses and activities as it is likely to function as an occasional event, performance and destination space. The space should also be the focus for active ground floor and cultural uses. It would provide an ideal frontage for a hotel. The space must be considered in conjunction with the existing Campshires. The design of the space must address the potential conflict of pedestrian priority, public transport movements and local access. The space will provide a setting for the landmark tower and the detailed design of the space will need to consider microclimatic issues associated with the tower.

3.3.4 Conservation designations and heritage

The Campshires will be preserved and protected in accordance with their policy and statutory requirements. The quay walls and remnant features of the waterfront on Britain Quay will be restored as part of the extension of the Campshires. The removal of the protected Hailing Station is sought in the Master Plan. In general terms, existing building lines will be retained. In Option 1 the existing street alignment of Britain Quay will be retained. In the case of Option 2 the alignment of Britain Quay will be relocated to the waterfront. Existing traditional street surfaces and features will be retained or reused as appropriate.



3.4 Transportation

3.4.1 Promoting sustainable transport

The proposed development will need to prioritise sustainable modes of transport. The public space will need to be designed to prioritise pedestrian movement. Adequate facilities will need to be provided for cyclists. The Authority will require a mobility management plan (MMP) if a development is likely to generate 500 or more vehicle trip movements per day or more than 100 vehicle trip movements in peak period. Developers will be required to consult with the Authority regarding the nature and content of the MMP. Reservations for on-street public transport shall be retained on Sir John Rogerson's Quay. Details of public transport services and connections to Poolbeg/Ringsend and the city centre via the Dodder Bridge will be the subject of agreement with the relevant authorities



3.4.2 Public transport bridge

The proposed public transport bridge will span the confluence of the Dodder and the Grand Canal Dock, springing from Britain Quay and landing at York Road in Ringsend. The Bridge will be designed solely for use by pedestrians, cyclists, taxis and public transport. The existing slipway on the eastern side of the Dodder shall be relocated within the immediate vicinity as part of the bridge proposal.

3.4.3 Parking

Parallel on-street car parking will be provided along the Sir John Rogerson's Quay building frontage and on the northern end of Benson Street. All other car parking associated with the development will be provided in underground parking areas. The level of provision will be as specified in the Planning Scheme.

3.4.4 Development access

General vehicular two-way access will be provided around the perimeter of the area. Vehicular access into the development will be provided at Benson Street and Green Street East. Business, service and loading access will be from the street. In Option 1, Britain Quay will be retained as a pedestrian priority route and for local vehicular access only.



3.5 Infrastructure

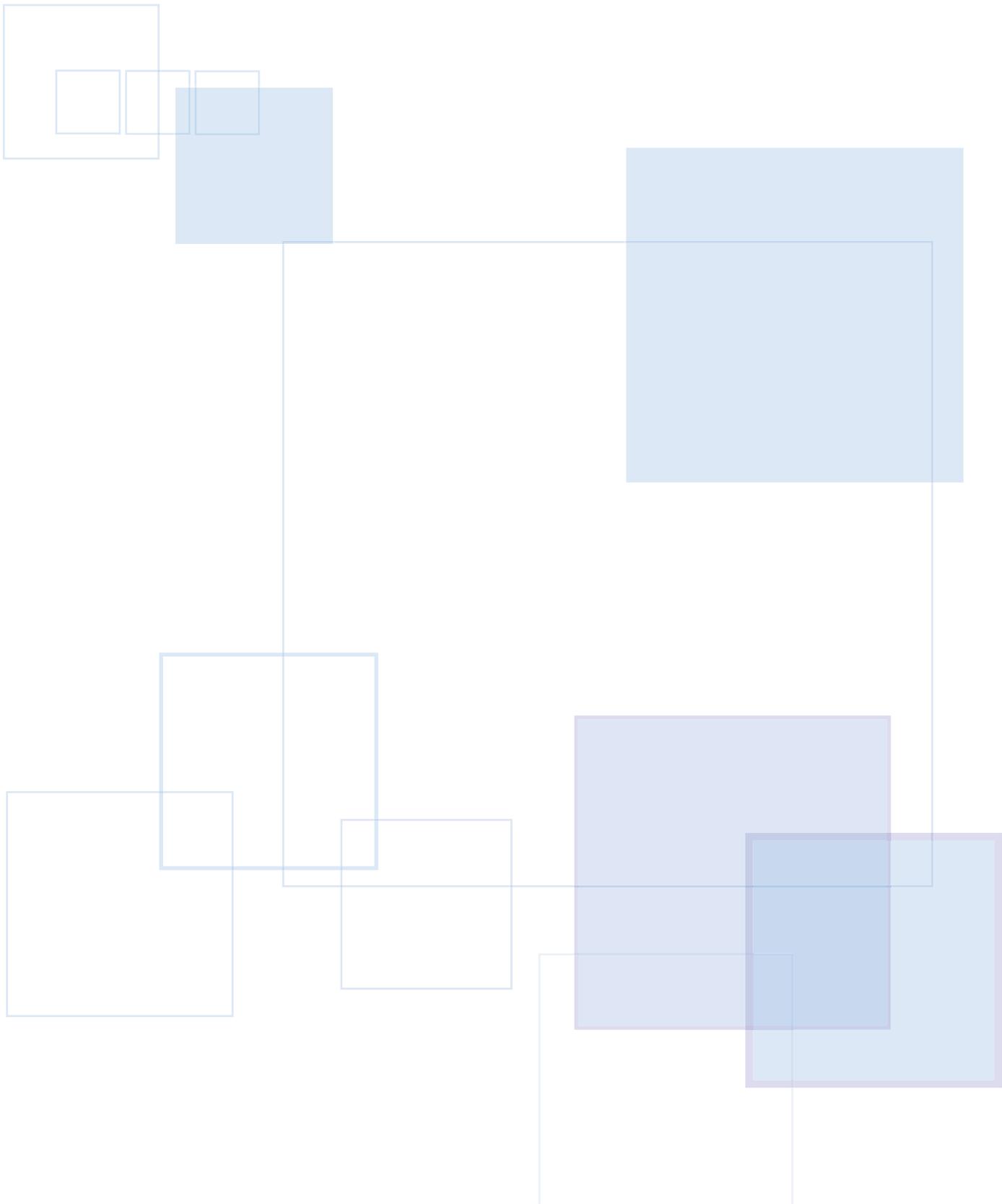
3.5.1 Services

The proposed development will have regard to a proposed services tunnel between Sir John Rogerson's Quay and York Road. Developers will be required to liaise with Dublin City Council regarding its location and any relevant requirements. The watermain, sewer and surface drainage networks will have to be upgraded in consultation and agreement with Dublin City Council.

3.5.2 Remediation

The lands will be remediated and cleaned to internationally acceptable standards prior to redevelopment.





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